

# AFFORDABLE HOUSING DESIGN GUIDELINES



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# **AFFORDABLE HOUSING DESIGN GUIDELINES**

(For the Construction and Rehabilitation of Single-Family Homes and Multi-family Units).

## **Purpose**

The City of Winston-Salem's Affordable Housing Design Guidelines provide a standard framework for clients utilizing city funding for the construction or rehabilitation of non-occupied single-family and multi-family housing. The purpose of the Guidelines is to foster a positive public perception of affordable housing by promoting the rehabilitation and/or design of single-family homes and duplexes in a manner that complements surrounding single-family housing styles, encourages a streetscape which promotes neighborhood interaction, and ensures a quality design that is less apt to experience functional obsolescence. Affordable housing design guidelines, while general enough to allow for unique design elements, promote structures that complement traditional urban development patterns rather than strictly adhering to contemporary setback and impervious surface ratios.

## **Application and Review**

Prior to loan approval for the construction or rehabilitation of a new single-family home or multi-family units funded by the City of Winston-Salem, a site plan with full setbacks of building floor plans, including building elevations for all proposed structures, shall be submitted to the Housing Development staff for initial review.

The Housing Development staff shall review plans and shall consult with additional staff representatives of the Building and Planning departments to ensure conformance with the Design Guidelines.

***The Housing Development Department with the City of Winston-Salem has the right to accept or reject all submittals.***

**Provision A: Building Location**

All principal and accessory structures within a City of Winston-Salem funded development site shall be

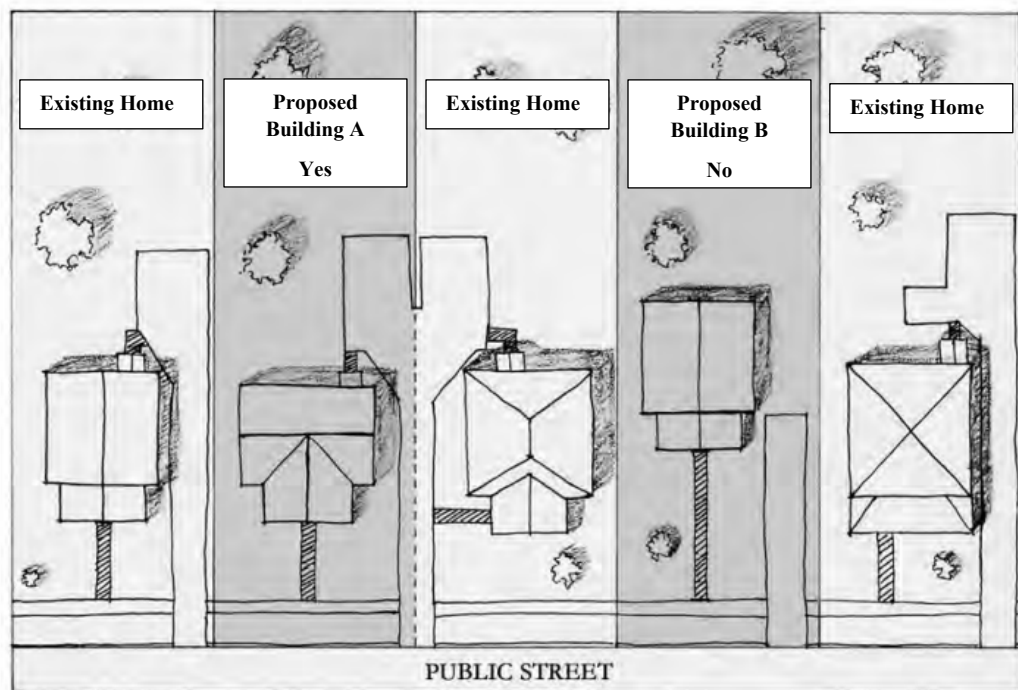
**Provision B: Pedestrian Access**

A minimum three (3) foot wide paved pedestrian path shall be provided from the primary pedestrian entrance of a single-family home to the driveway and/or sidewalk. Minimum three (3) foot wide paved pedestrian paths shall be provided from the primary pedestrian entrances of each unit in a duplex development to a driveway and/or sidewalk.

**Provision C: Building Entrance**

Each single-family home and duplex unit shall be provided with at least two (2) pedestrian entrances to promote pedestrian flow through all portions of the unit and lot unless the developer can illustrate that a second pedestrian entrance is not feasible.

**Figure 1:**  
Building Location,  
Pedestrian Access,  
& Building  
Entrance.



Proposed Building A is consistent with the Affordable Housing Design Guidelines. It is set back from the lot lines in a manner consistent with adjacent existing homes. Proposed Building A also provides direct pedestrian access to the public sidewalk and street and includes a second pedestrian access to the rear yard. While proposed Building B provides pedestrian access to the sidewalk and street, it does not adhere to the Guidelines because it draws attention to itself and breaks up the streetscape by being located much further back than surrounding existing homes. Also, the lack of a second pedestrian access, especially on narrow lots, can decrease the use and enjoyment of the entire lot.

## **Provision C: Building Entrance (Cont.)**

**Single Family** – The primary pedestrian entrance to a new single-family residential home, or home to be rehabilitated shall face the street.

**Duplex** – The primary pedestrian entrance to at least one of the units in a new duplex, or a duplex to be rehabilitated, shall face the street. The primary pedestrian entrances for both dwelling units may face the street, but only if all other elements of the street facing façade give the impression of a single-family structure. If possible, incorporate the street facing access for both dwelling units into one (1) shared entrance, a new duplex, or a duplex to be rehabilitated, shall face the street. The primary pedestrian entrances for both dwelling units may face the street, but only if all other elements of the street facing façade give the impression of a single-family structure. If possible, incorporate the street facing access for both dwelling units into (2) shared entrance.

**Figure 2:**  
Building  
Entrance



**Yes.** All entry features of the home address the street



**Yes.** While the front door of this home does not face the street, the overall orientation of the entry way elements are forward facing.

**Figure 3:**  
Building  
Entrance



**Yes.** Both units within this duplex have their own entrance but all other building elements emulate a single-family home, including size and scale.

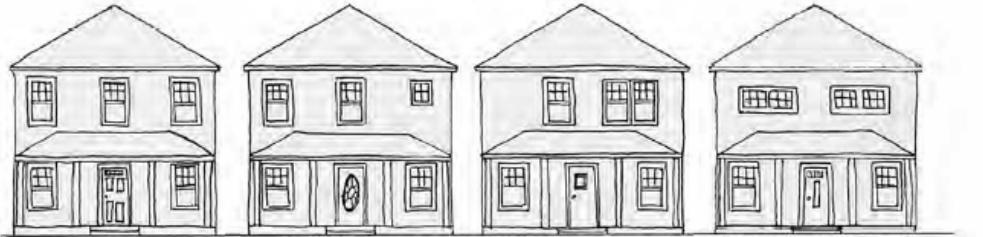


**No.** The doors of these duplex units face the side yard instead of the street. The side of the building is orientated toward the street.

### **Provision D: Windows**

On facades which face the street or other public spaces, window openings shall be similar in rhythm, size, and proportion to that of most single-family homes in the vicinity - particularly structures directly adjacent to the subject property. On structures to be rehabilitated, every effort shall be made to maintain the original proportions of windows which face the street or other public spaces.

**Figure 3:**  
Windows



**Yes.** All windows on this home are a consistent size and are evenly spaced.

**No.** The window in the upper right corner is much smaller than other windows. Such a situation can be a result of a floor plan that places a bathroom or kitchen against the front building façade.

**No.** The lack of rhythm in the space of the second-floor windows results in an unbroken expanse of siding on the front building façade.

**No.** The vertical windows on the first floor and the horizontal windows on the second floor clash and create an inconsistency of style.

### **Provision E: Individual Identity**

To the extent possible, single-family homes and duplexes constructed adjacent to, or in proximity of, each other should exhibit unique characteristics – regardless of whether they utilize the same floor plan. Acceptable alternatives may include varying roof pitches and orientation, window arrangement, porch arrangement, varying materials, etc. so long as such alternatives do not conflict with the other design elements addressed within this document.

**Figure 4:**  
Individual Identity



**While a consistent size and scale is exhibited by these homes utilizing the same floor plan, slight variations in the roof, exterior building material, details, windows, and door give every unit a distinctive identity.**

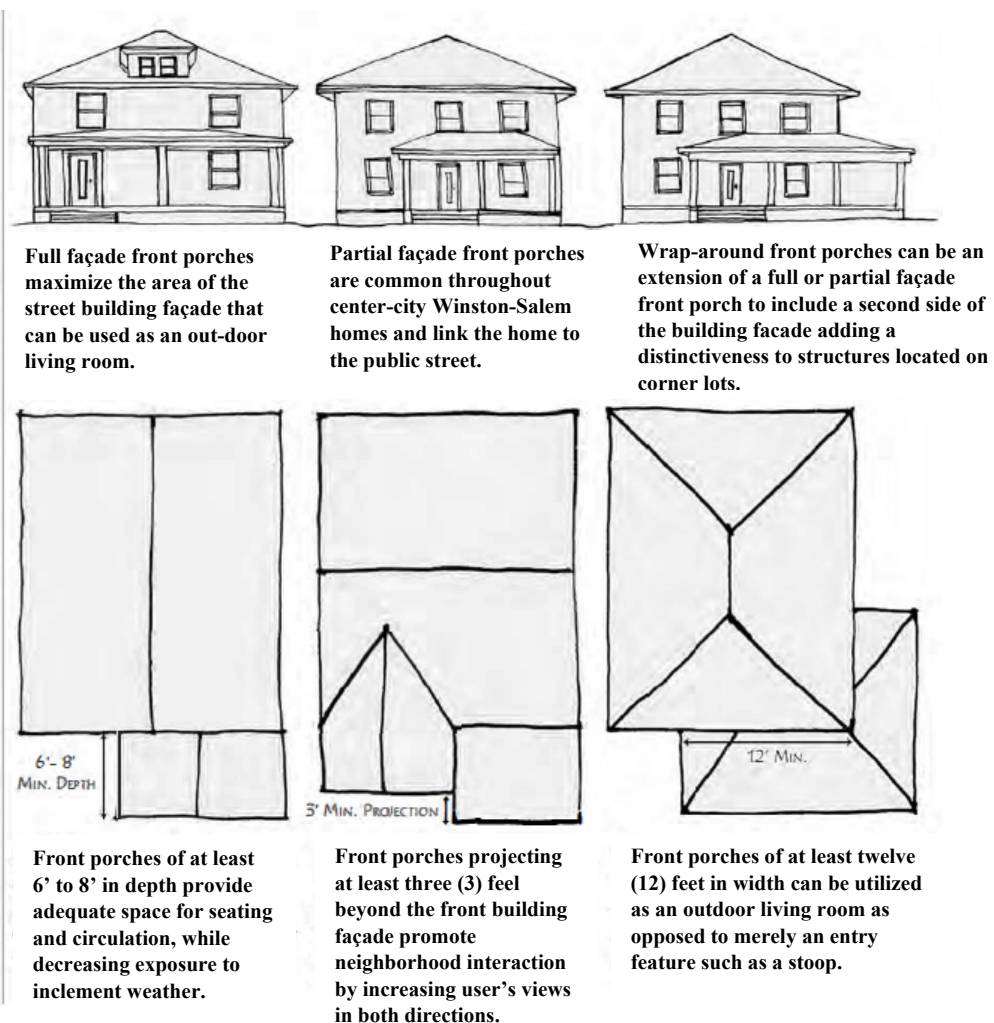
## **Provision F: Porches**

1. Single-family residential homes and duplexes shall include a front porch. The front porch of structures to be rehabilitated shall be retained or replicated in a manner that is consistent with the original. Front porches on new construction shall be a full facade, partial facade or wrap around porch depending on the design of single-family homes in the vicinity. Alternative porch arrangements may be considered where no predominant style exists. A porch may be omitted from the front building facade only if such an omission is necessary to make the unit consistent with surrounding homes of significant architectural style.

2. Front porches shall be at least six (6) to eight (8) feet in depth; and, at a minimum shall span at least twelve (12) feet in width Or approximately fifty (50) percent of the front building facade, whichever is greater.

3. Partial facade front porches shall project at least three (3) feet beyond the front building facade unless the developer can illustrate that such a projection is not feasible.

**Figure 4:**  
Porches

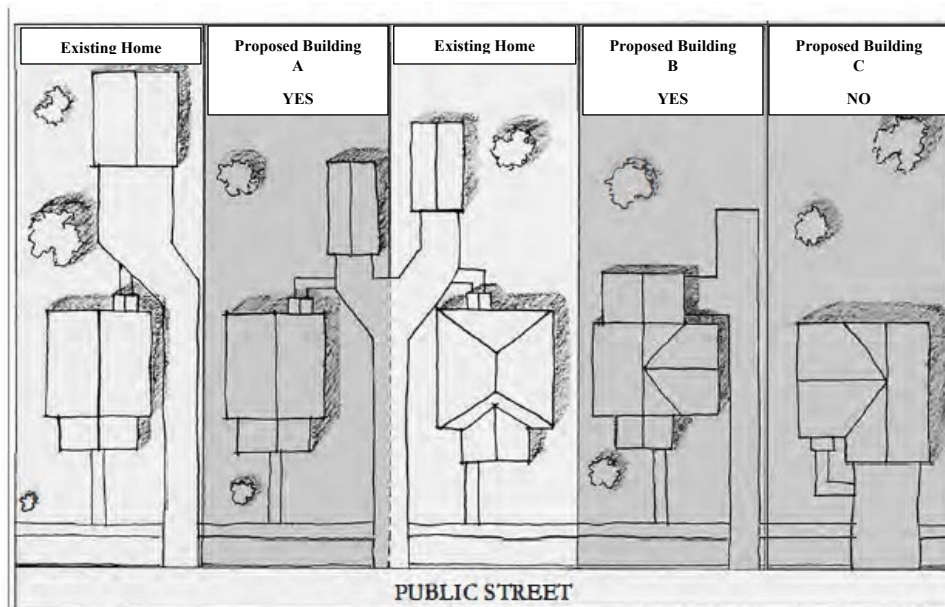




## **Provision G: Garages**

1. A garage is not a required building element. When utilized, attached, or detached garages shall be located on the parcel in a manner consistent with most single-family homes in the vicinity, except that it is preferable to remove the garage from the front building facade where possible.
2. Attached garages located on front building facades shall be recessed a minimum of three (3) feet behind the front building line (including the porch) and shall not occupy more than fifty (50) percent of the overall facade area.
3. Detached garages shall be of a similar height of garages on adjacent lots, shall have a roof pitch similar to that of adjacent garages and shall be of a scale that is accessory to the principal residential structure.

**Figure 7:**  
Garages



The rear detached garage utilized by proposed Building A is consistent with existing homes in the neighborhood. The rear attached garage utilized by proposed Building B is also consistent with the Guidelines. Recessing the garage from the side façade suggests that the garage is an addition and limits its view from the street and decreases the lineal dimension of the home. The garage proposed for Building C is not consistent with the Guidelines because no effort has been made to remove it from the front building facade; or de-emphasize its presence on the street.

**Figure 8:**  
Garages



**Yes.** When permitted, attached garages located on the front of the building should be de-emphasized to avoiding street-scapes that are dominated by wide blank facades.



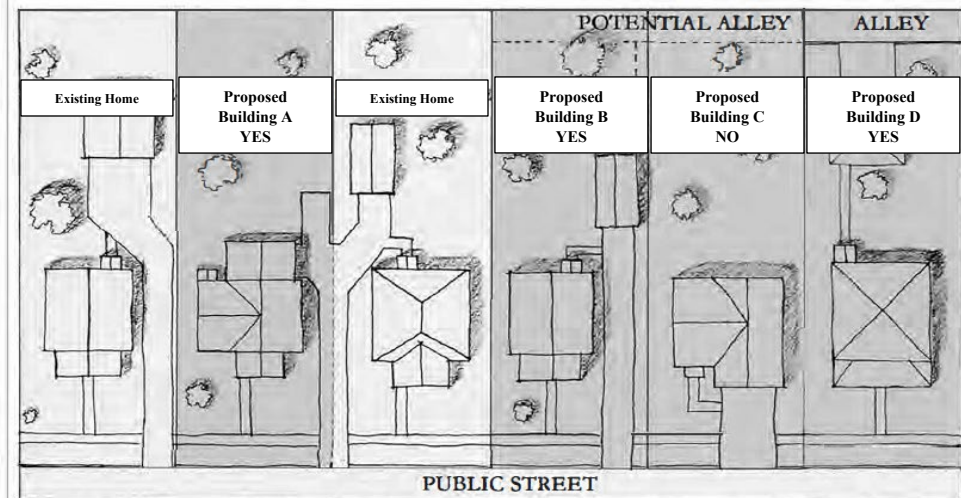
**No.** Attached garages that span more than fifty (50) percent of the front building façade and/or project toward the street dominate the streetscape and remove residents' eyes from the street – decreasing awareness of surrounding activities.



## **Provision H: Vehicular Access**

Where possible, vehicular access to the development site shall be provided via an alley. The use of shared driveways is not acceptable with program guidelines with a single-lane wide approach should also be considered to decrease the width of curb cuts on the public street.

**Figure 9:**  
Vehicular Access

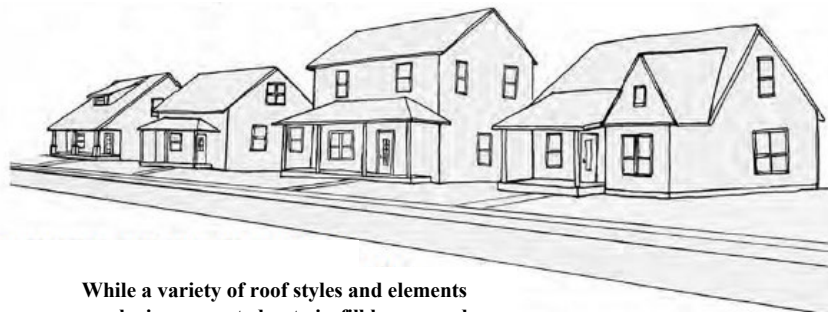


Vehicular access to proposed Buildings A & B is consistent with the Affordable Housing Design Guidelines because both scenarios lead to parking areas placed to the rear of the homes. As illustrated by proposed Building A and B, a shared access driveway is NOT acceptable. If possible, access to parking areas should be provided via an alley as illustrated by proposed Building D. Alley access decreases the amount of paving necessary to reach rear parking areas. Removal of the curb cut from the street also increases on-street parking opportunities. Vehicular access to proposed Building C is not consistent with the Guidelines because the wide curb cut results in a streetscape that is dominated by impervious surface area while increasing storm water run-off into the public street

## **Provision I: Roof**

1. The principal residential structure's roof pitch, and orientation of the roof ridge shall be compatible with single-family homes in the vicinity. This requirement does not disallow roof line alterations, if necessary, for structures subject to rehabilitation so long as the alteration is reasonably consistent with the surrounding neighborhood.
2. The roof style, including gables and dormers, shall be consistent, with single-family homes in the vicinity. False dormers may be permitted to ensure the consistency of roof style.

**Figure 10:**  
Roof



While a variety of roof styles and elements can be incorporated onto in-fill homes and duplexes, ridge orientation and pitch should be consistent with existing homes.

## **Provision J: Building Massing and Scale**

As viewed from the street on which the front entrance is oriented, the proportion of the building height to width should be compatible with the same proportions of the other buildings on the block. Generally, floor to floor height of new structures should relate to adjacent single-family homes: and the overall height at the eaves should relate to dwelling units within close proximity.

**Figure 11:**  
Building Massing  
and Scale



The highlighted home is inconsistent with surrounding units due to its horizontal dimensions and noticeably shorter eave heights.

## **Provision K: Exterior Building Materials**

Exterior surface materials, excluding the roof, shall be compatible with that of other buildings on the block except for the Development staff may permit the use of exterior surface materials such as vinyl siding, Hardie board siding, or faux stone etc., that complement those of the majority of single-family homes surrounding the lot.

**Figure 12:**  
Exterior Building  
Materials



While the new home in the center primarily utilizes siding as opposed to the surrounding brick homes, the application of brick veneer along the base of the building façade and columns balances the need to apply consistent building materials to new homes while ensuring that such application does not create an unacceptable increase in the cost of the unit.

## **Provision L: Utility Placement**

Electric and gas meters serving new construction shall not be placed on a street facing building facade. Every effort shall be made to re-locate electric and gas meters to a non-street facing building facade of a dwelling unit to be rehabilitated.

**Figure 13:**  
Utility Placement

**No.** The utility meters on this home have been placed on the front façade. They must be moved to at least one of the walls.



**Acceptable.** While still seen from the street, these utility connections have been de-emphasized by being placed to the side façade.



**Yes.** In this scenario, utilities can be accessed from an alley and utility meters are obscured by being located on the rear façade.

